

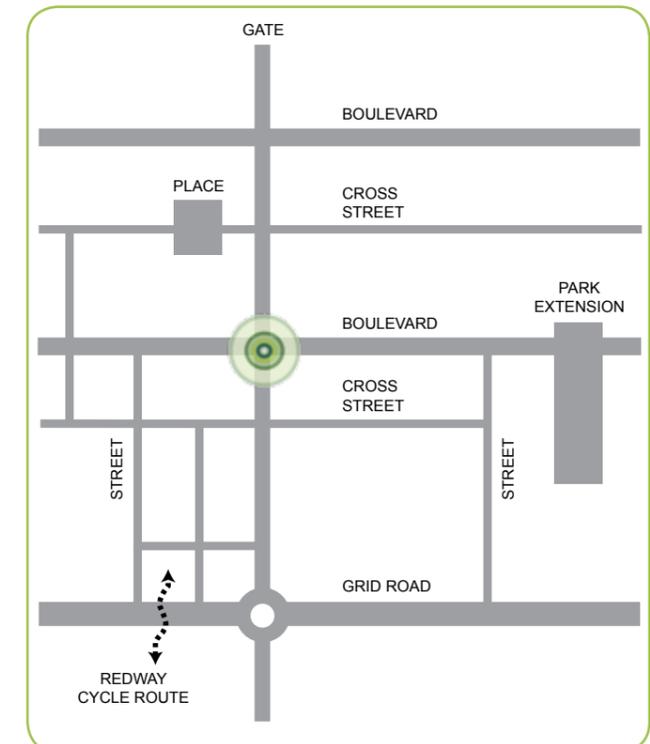
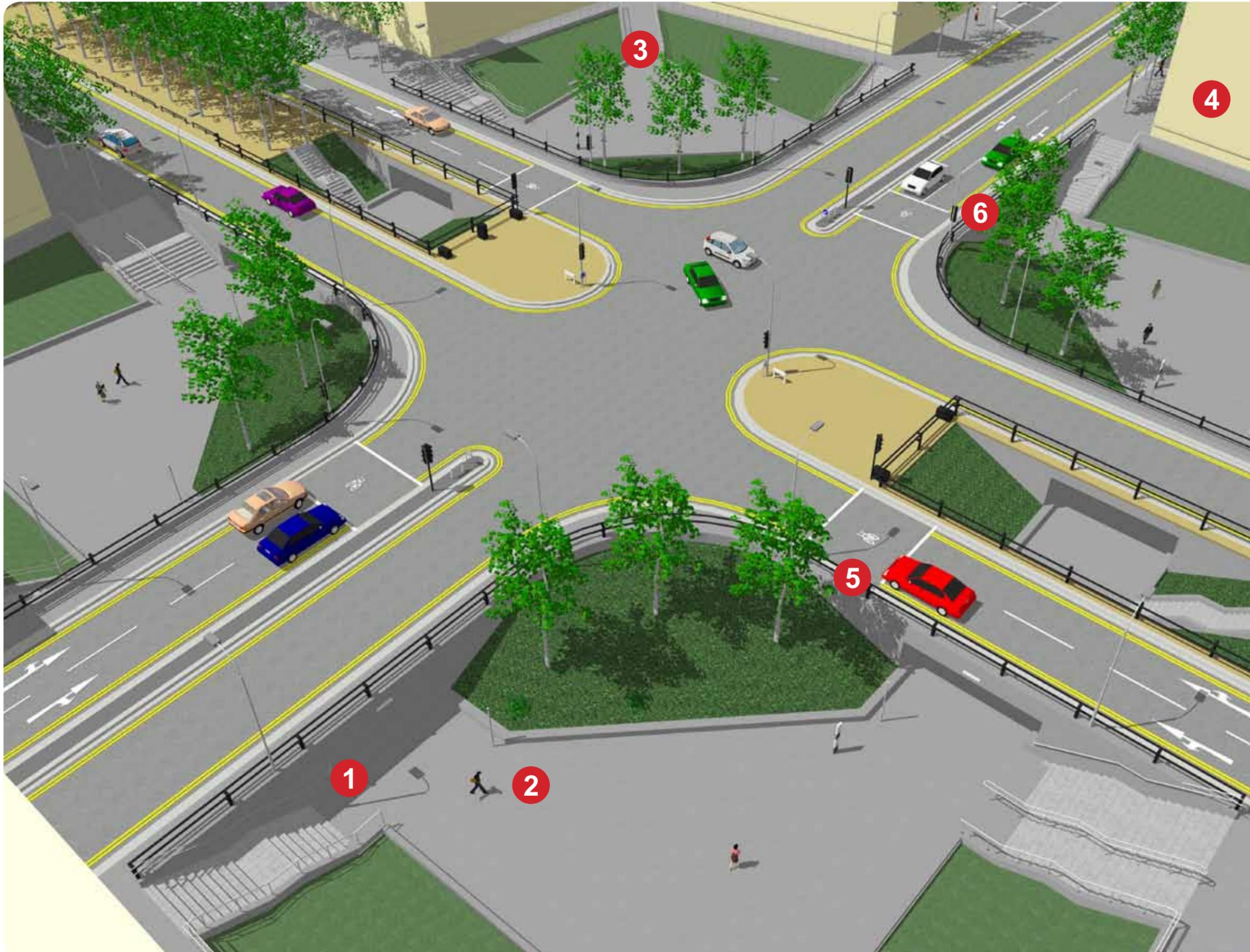
## The Main Features

- 1 Underpasses retained.
- 2 Assumes retention of existing underpass gradients.
- 3 New ramps and steps accessible & convenient.
- 4 Active building fronts.
- 5 Cycle priority.
- 6 Street furniture integrated at every opportunity.

## Gate / Boulevard junction

### Pedestrian underpass

Underpasses can be retained even when building lines come forward, as illustrated. The careful use of ramps and steps retain accessibility and ease of movement for all. Developers will need to respect the constraints on building footprints in order to achieve this quality of public realm.



# Gate / Boulevard junction

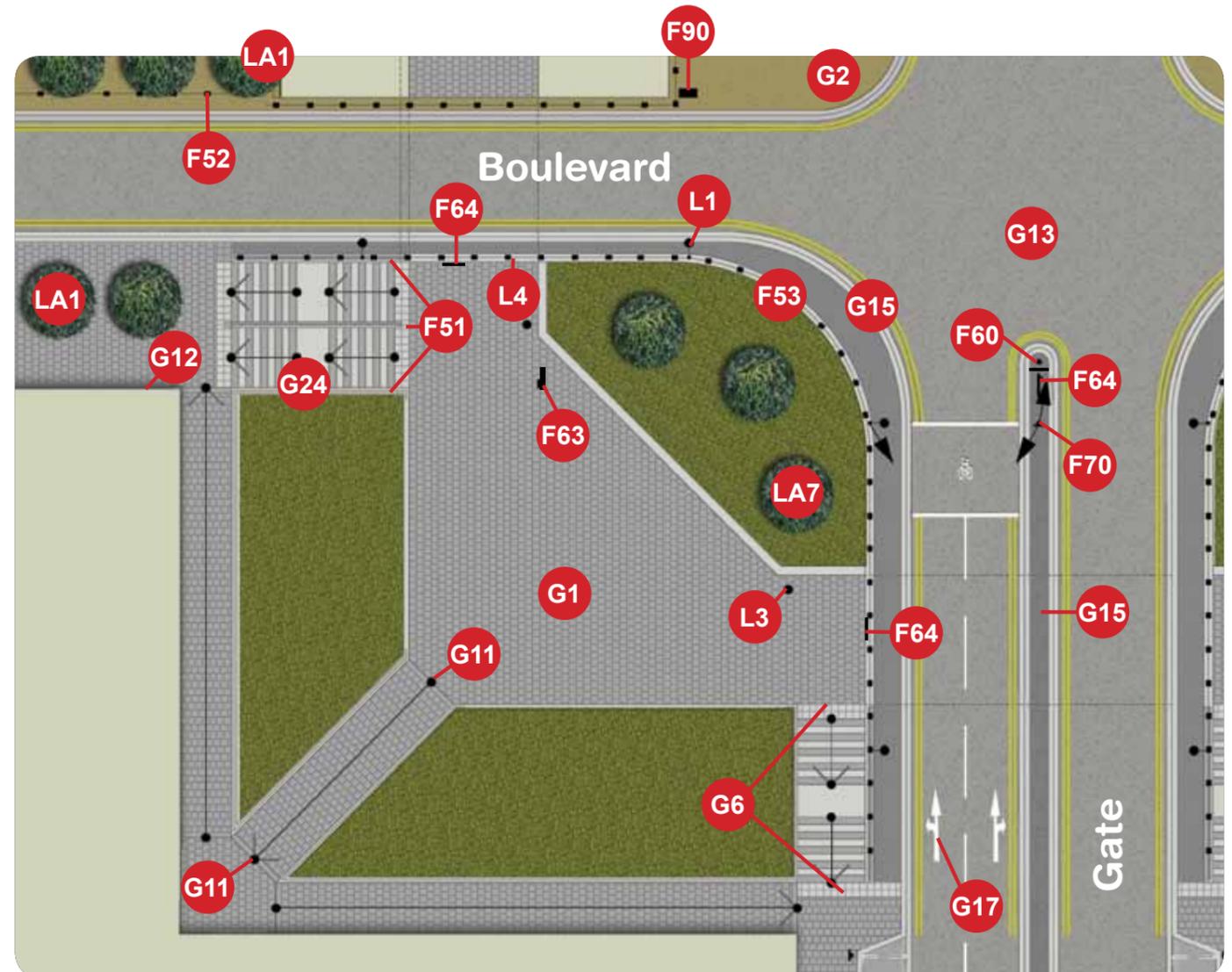
## Pedestrian underpass

### Setting out Principles

- Lighting - Positions as existing on bridge parapet.
- New ramps - Maximum slope 1 in 20.
- New ramps - Minimum width 3m.
- Steps - Half landing at mid point (where ten or more risers).
- Steps - Central balustrade (where steps are in excess of 3m wide)
- Steps - Width to suit building line.
- Footways- widths (from back of footway to front of kerb) are:
  - Boulevards 9.5m
  - Gates 9.5m
- Building line setbacks at corners are to be verified by developers.
- The planting design for the embankments and environment of the underpasses should be in accordance with the adopted technical guidance: Street Trees in CMK: Guidance on the development Process. Bold bands of colour planting, combined with contrasting evergreen and silver foliage plants in a formal style, will seek to provide striking and differing visual solutions to each location. Planting to have topsoil depth of 450mm.

### Public Realm Elements

- G1** Silver grey pre-cast concrete paving flags 400 x 400 stagger bond
- G2** Buff Breedon gravel
- G6** Precast concrete tonal contrast corduroy paving 400 x 400 stack bond – two rows at top and bottom of all steps (not required for ramps)
- G11** Light silver-grey granite kerbs to top and bottom of ramps – laid to ends on bottom and to faces on top
- G12** Drainage channels with bright steel grating
- G13** Black matrix hot-rolled asphalt
- G15** Granite sets 100 x 100 stack bond
- G17** Durable paint carriageway markings
- G24** Pre-cast concrete steps
- F51** Stainless steel hand rail – required for all steps
- F52** Painted black central reservation barrier
- F53** Junction parapet railing
- F60** Stainless steel with lacquered traffic sign frontlit
- F63** Steel street directory
- F64** Steel street name plate
- F70** Stainless steel with lacquer finish traffic signal pole with vehicle aspect
- L1** 8m tall painted and lacquered galvanised steel column with aluminium lantern
- L3** 3m tall painted and lacquered galvanised steel column with aluminium lantern
- L4** Tube/ strip high level light in underpasses
- LA1** London Plane *Platanus acerifolia* at 4.8m spacing
- LA7** Ornamental Cherry *Prunus* 'Pink Perfection' or similar as agreed.



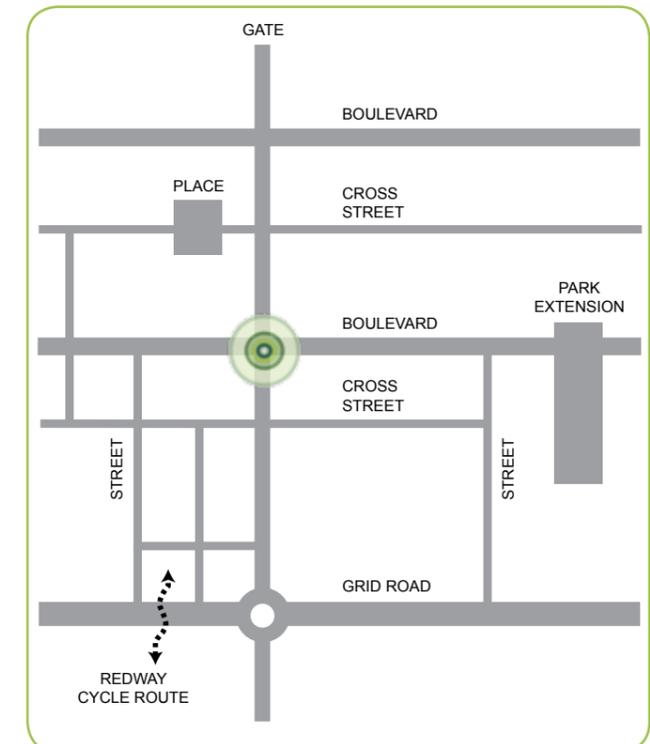
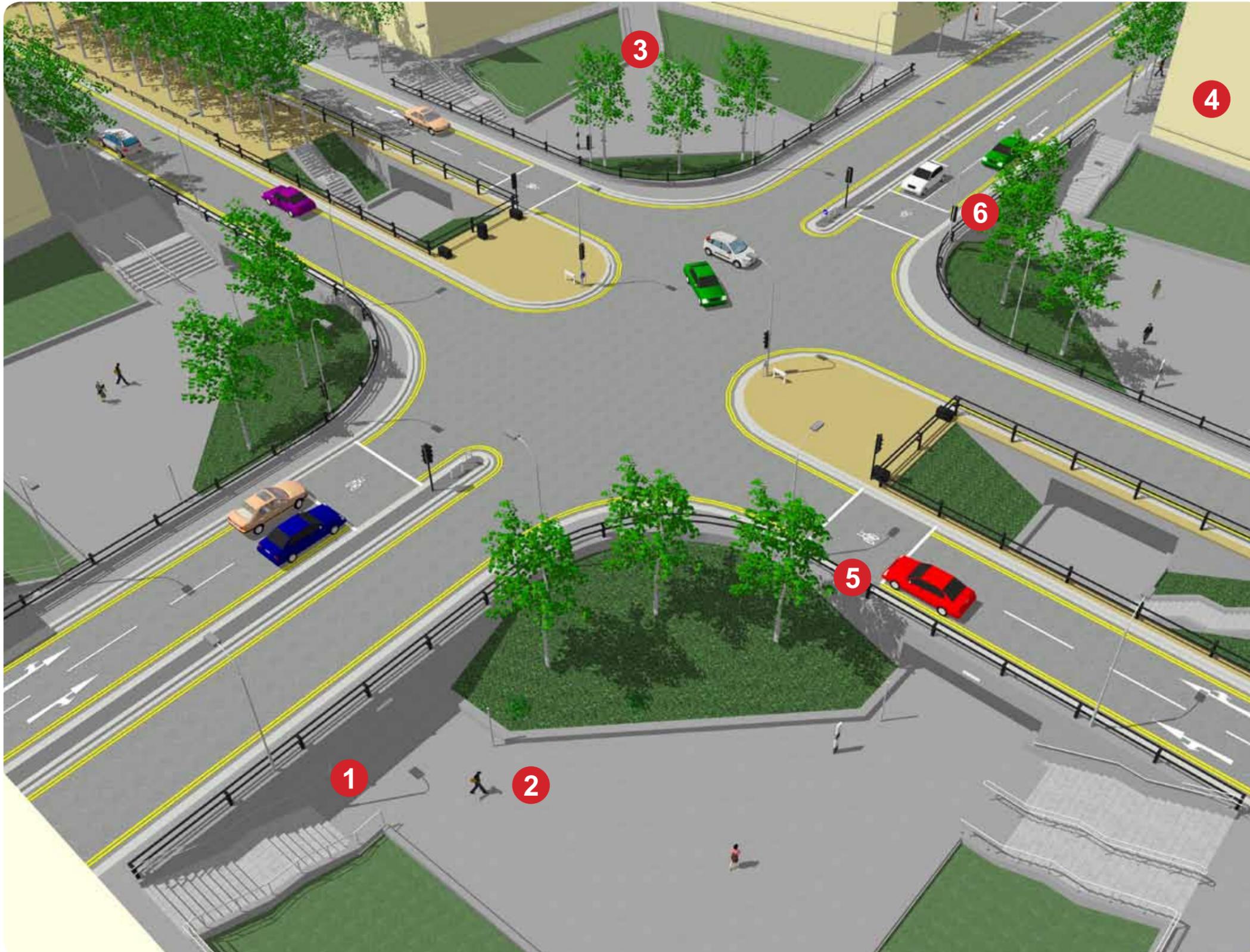
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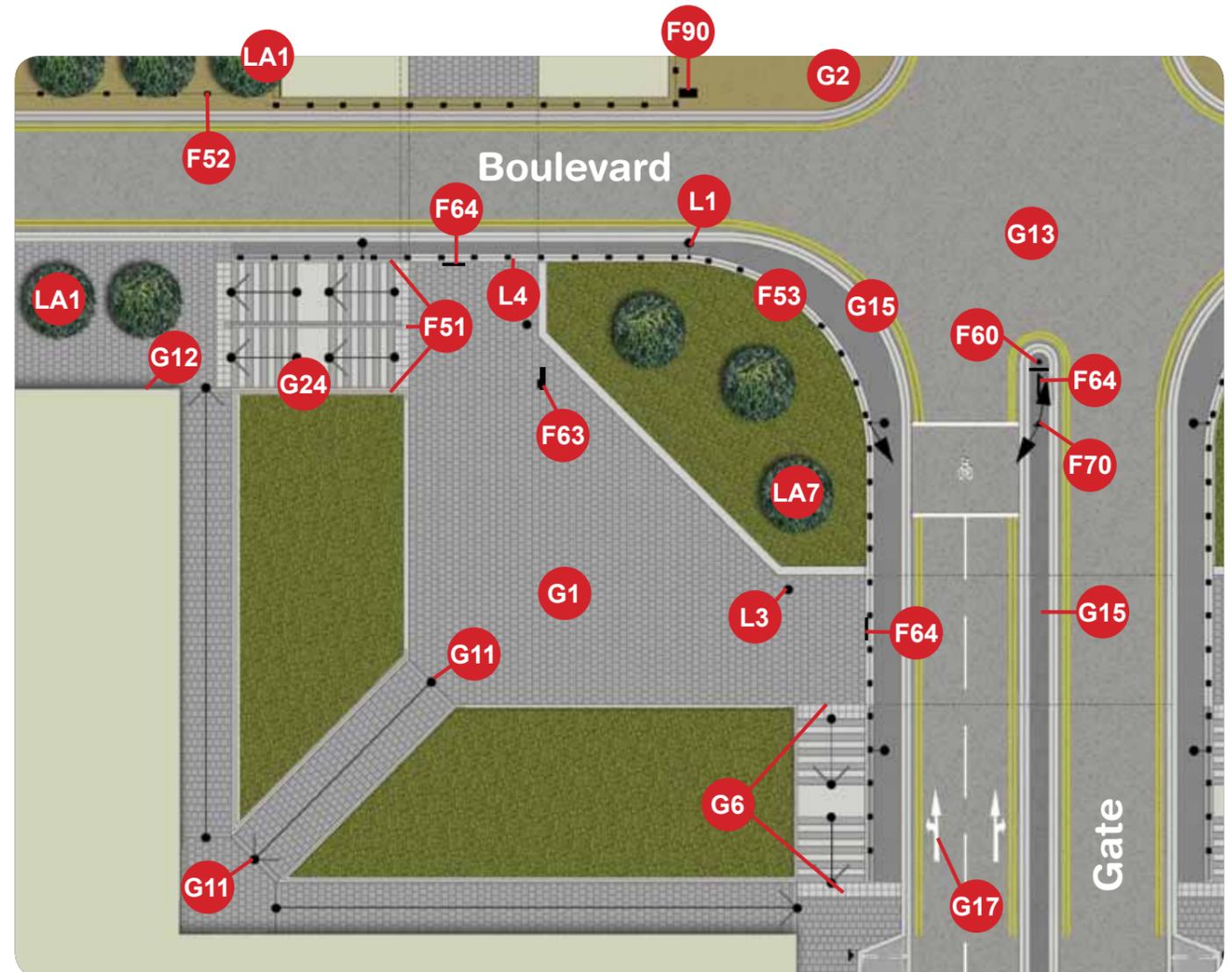
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- G12** Drainage channels with bright steel grating
- G13** Black matrix hot-rolled asphalt
- G15** Granite sets 100 x 100 stack bond
- G17** Durable paint carriageway markings
- G24** Pre-cast concrete steps
- F51** Stainless steel hand rail – required for all steps
- F52** Painted black central reservation barrier
- F53** Junction parapet railing
- F60** Stainless steel with lacquered traffic sign frontlit
- F63** Steel street directory
- F64** Steel street name plate
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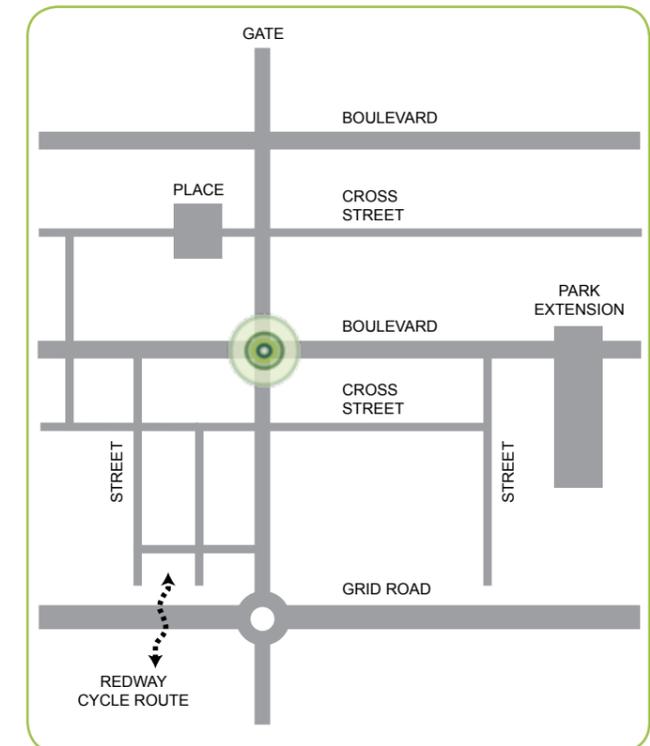
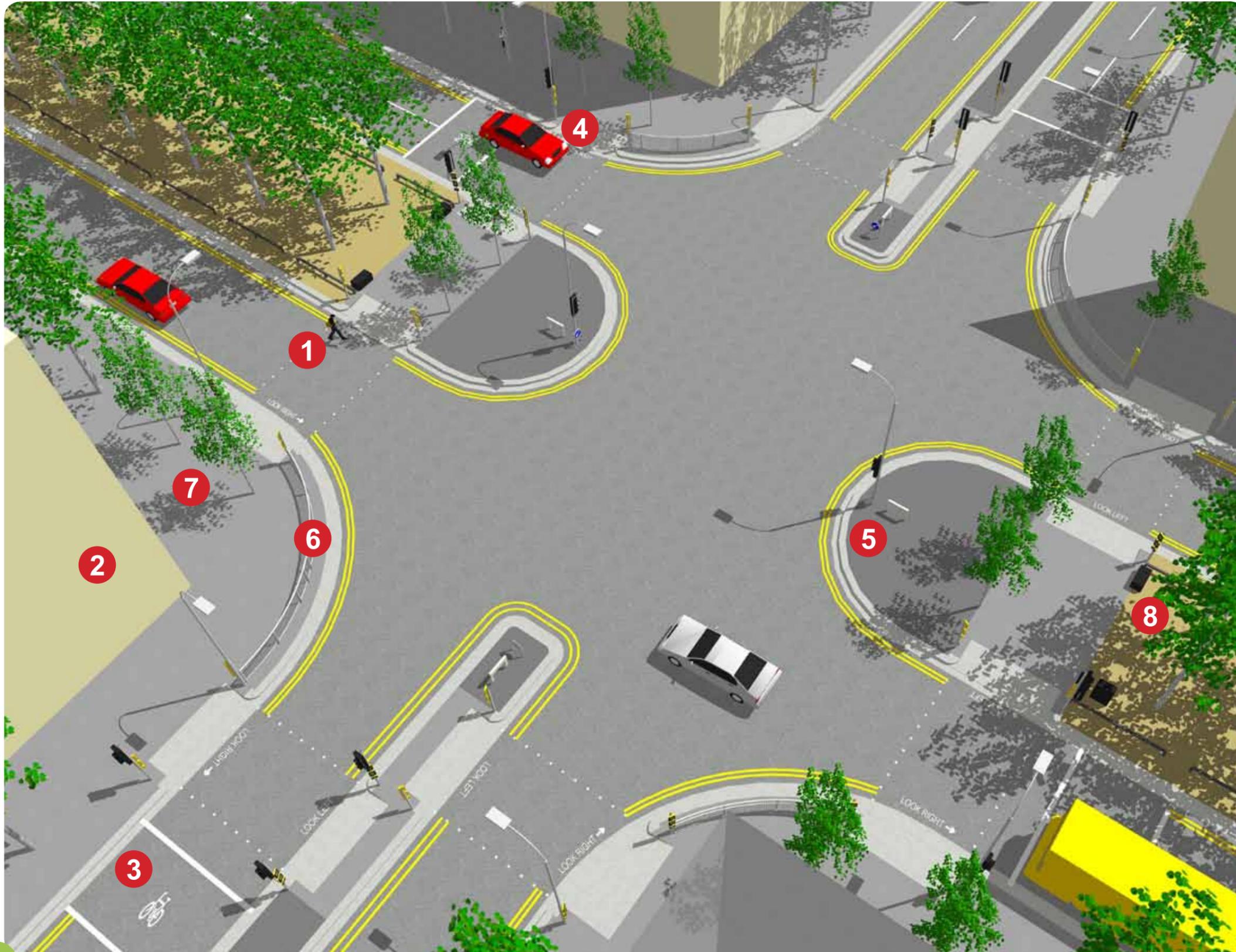


## The Main Features

- 1 Convenient routes on foot.
- 2 Active building fronts.
- 3 Cycle priority.
- 4 Street furniture integrated at every opportunity.
- 5 Symmetrical lighting layout achieved.
- 6 Minimum guard railing.
- 7 Trees extend up to the junction.
- 8 Control boxes positioned clear of pedestrian movements.

## Gate / Boulevard junction Straight Pedestrian Crossings at Grade

Where pedestrians cross at ground level, the careful positioning of crossings means that lights, traffic signals and pedestrian facilities can be aligned, minimising street clutter. The advanced building line creates a more traditional feel to the street.



# Gate / Boulevard junction

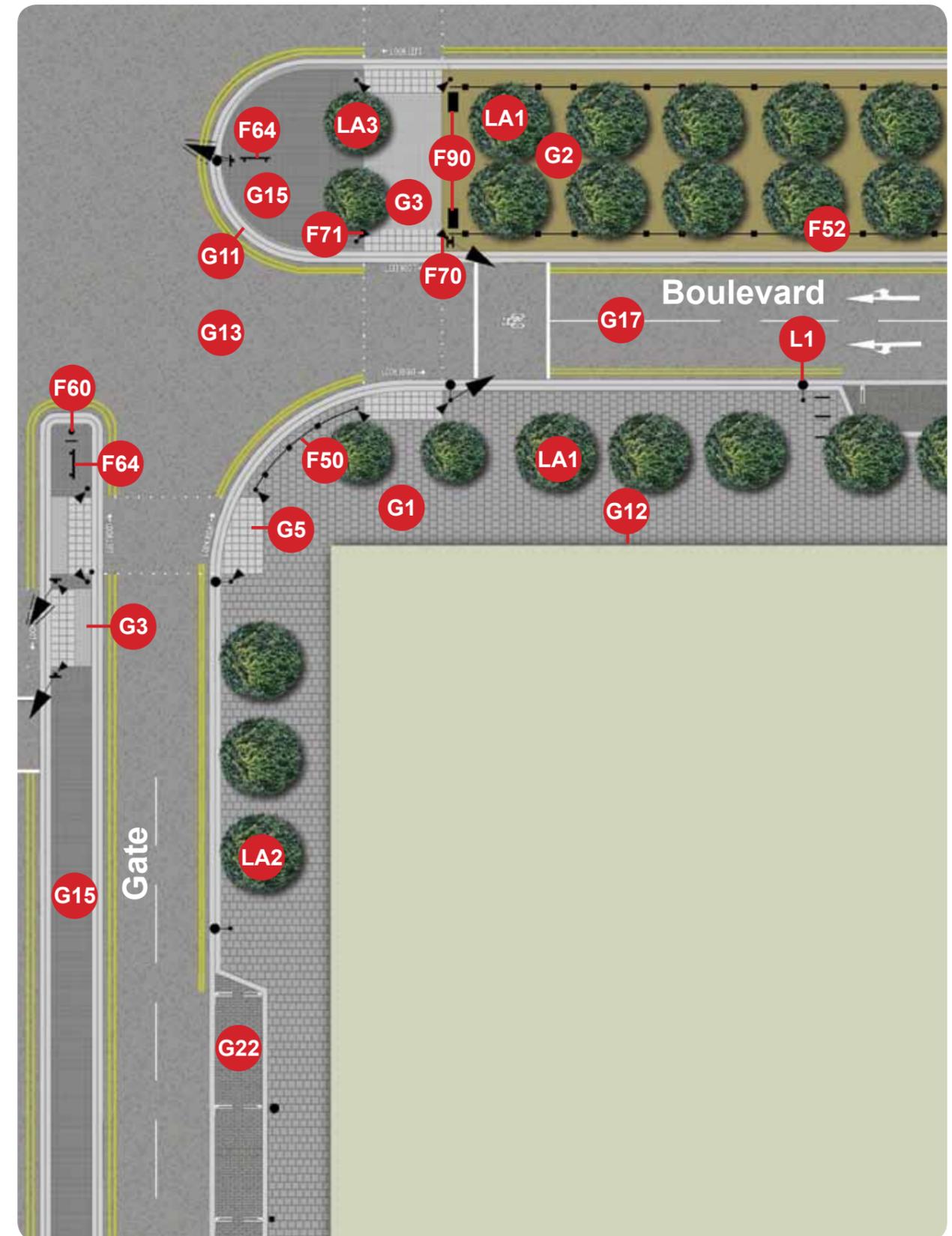
## Straight Pedestrian Crossings at Grade

### Setting out Principles

- Lighting - 0.8m offset to kerb to suit traffic signal equipment (where mounted).
- Lighting – positioned symmetrically on kerbside of footways.
- Pedestrian crossings – 4m wide.
- Straight pedestrian crossings – set out on the far side of line of existing lighting columns.
- Boulevard central reservations – extended to position new light columns in line with other light columns.
- Gate central reservation – minimum 2.5m wide.
- Traffic signal equipment – on central reservations, clear of line of pedestrian crossings.
- Keep left signs – on centre-line of central reservations.
- Footways- widths (from back of footway to front of kerb) are:
  - Boulevards 9.5m
  - Gates 9.5m
- Footway widths allow for 2.4m width for lay bys.
- Building line setbacks at corners are to be verified by developers.

### Public Realm Elements

- G1** Silver grey pre-cast concrete paving flags 400 x 400 stagger bond
- G2** Buff Breedon gravel
- G3** Concrete setts 100 x 100 stagger bond
- G5** Precast concrete tonal contrast tactile paving 400 x 400 stack bond – three full rows and no tails
- G11** Light silver-grey granite kerbs
- G12** Drainage channel with bright steel grating
- G13** Black matrix hot-rolled asphalt
- G15** Granite setts 100 x 100 stack bond
- G17** Durable paint carriageway markings
- G22** Pre-cast concrete block Layby & Taxi pick up
- F50** Painted black guard rail
- F52** Painted black central reservation barriers
- F60** Stainless steel with lacquer finish traffic sign frontlit
- F64** Steel street name
- F70** Stainless steel with lacquer finish traffic signal pole with near-side vehicle aspects
- F71** Stainless steel with lacquer finish pole with near-side pedestrian aspects
- F90** Painted black traffic signal control boxes
- L1** 8m tall painted and lacquered galvanised steel column with aluminium lantern
- LA1** London Plane *Platanus acerifolia* at 4.8m spacing
- LA2** Fastigate Hornbeam *Carpinus betulus* 'Frans Fontaine' at 4.8m spacing
- LA3** Ornamental Pear *Pyrus calleryana* 'Chanticleer' or similar as agreed.



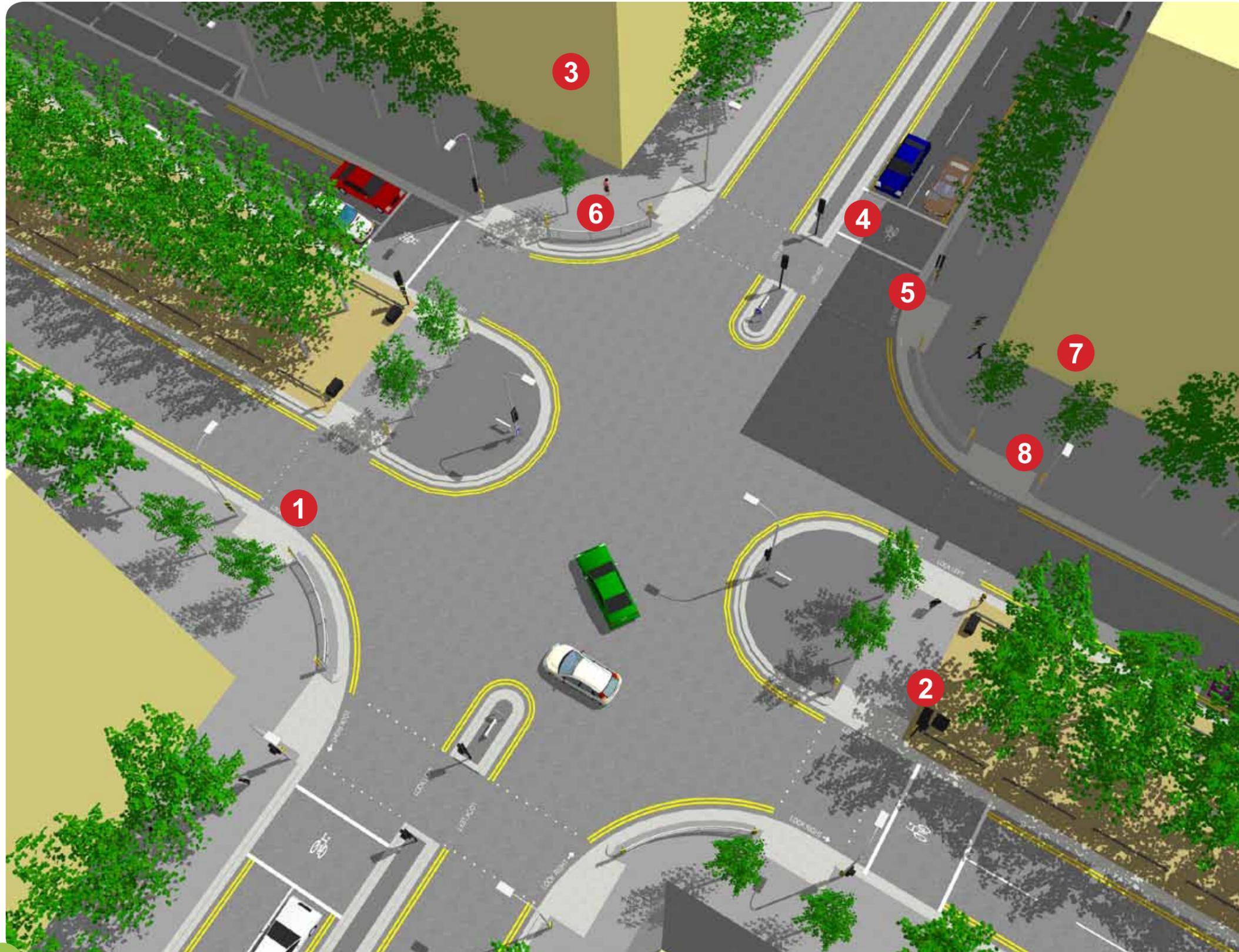
## The Main Features

- 1 Direct routes on foot.
- 2 'All-red' signal phases necessary.
- 3 Active building fronts.
- 4 Cycle priority
- 5 Street furniture integrated at every opportunity.
- 6 Minimum guard railing.
- 7 Trees extend up to the junction.
- 8 Symmetrical lighting layout achieved.

## Gate / Boulevard junction

### Straight Pedestrian Crossings at Grade

In this example, all pedestrian crossings are straight. Traffic would need to be stopped long enough for pedestrians to cross the Gates in one stage. The number of pedestrian facilities required is minimised, and achieves the cleanest possible visual appearance.



# Gate / Boulevard junction

## Straight Pedestrian Crossings at Grade

### Setting out Principles

- Lighting - 0.8m offset to kerb to suit traffic signal equipment (where mounted).
- Lighting – positioned symmetrically on kerbside of footways.
- Pedestrian crossings – 4m wide.
- Pedestrian crossings – set out on the far side of line of existing lighting columns.
- Boulevard central reservations – extended to position new light columns in line with other light columns.
- Gate central reservations – minimum 1.5m wide.
- Traffic signal equipment – on central reservations, clear of line of pedestrian crossings.
- Keep left signs – on centre-line of central reservations.
- Footways-widths (from back of footway to front of kerb) are:
  - Boulevards 9.5m
  - Gates 9.5m
- Footway widths allow for 2.4m width for lay bys.
- Building line setbacks at corners are to be verified by developers.

### Public Realm Elements

- G1** Silver grey pre-cast concrete paving 400 x 400 flags stagger bond
- G2** Buff Breedon gravel
- G3** Concrete setts 100 x 100 staggerbond
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- G11** Light silver-grey granite kerbs
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- F52** Painted black central reservation barriers
- F60** Stainless steel with lacquer finish traffic sign frontlit
- F64** Steel street name
- F70** Stainless steel with lacquer finish traffic signal pole with near-side vehicle and pedestrian aspects
- F71** Stainless steel with lacquer finish traffic signal pole with near-side pedestrian aspects
- F90** Painted black traffic signal control boxes
- L1** 8m tall painted and lacquered galvanised steel column with aluminium lantern
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