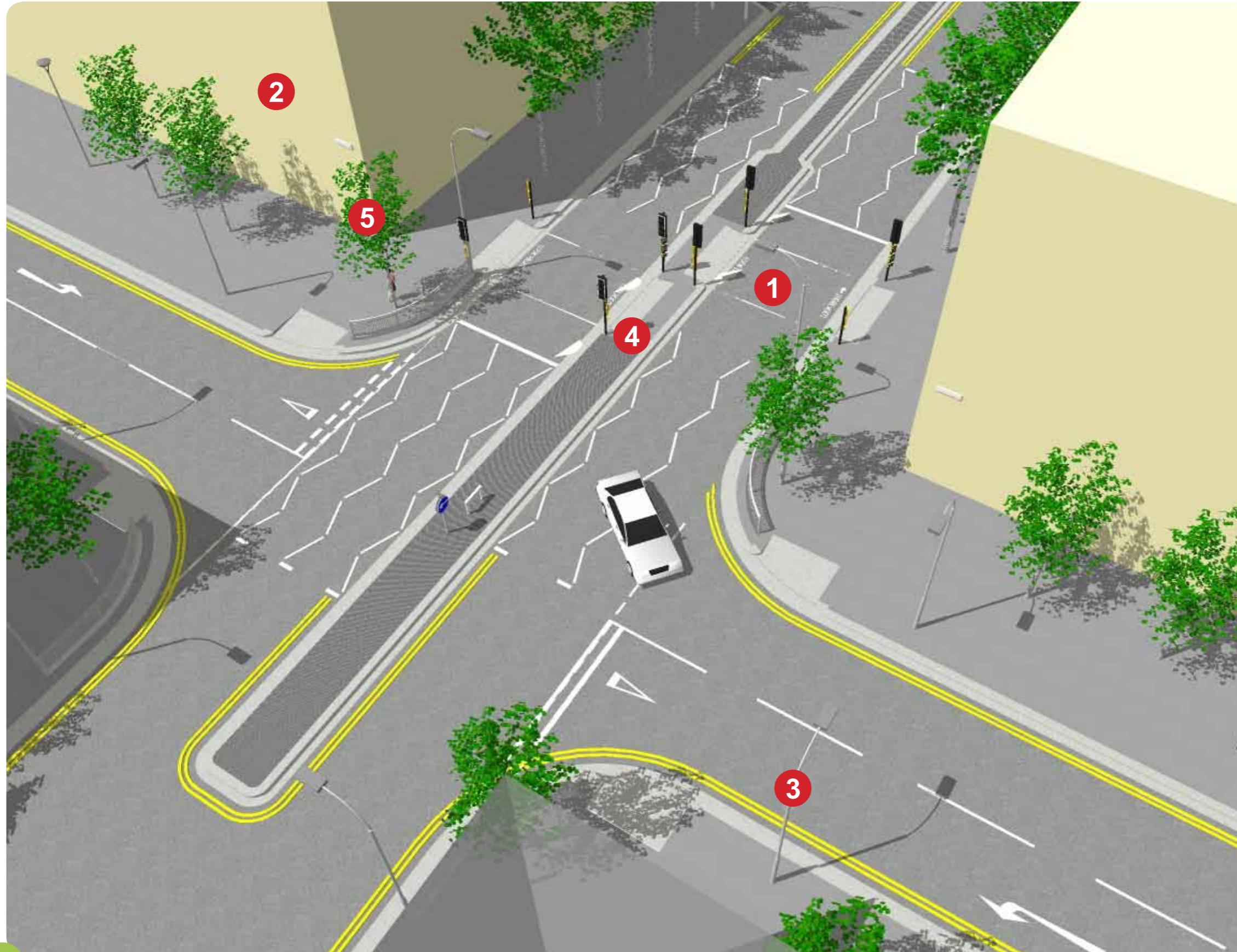


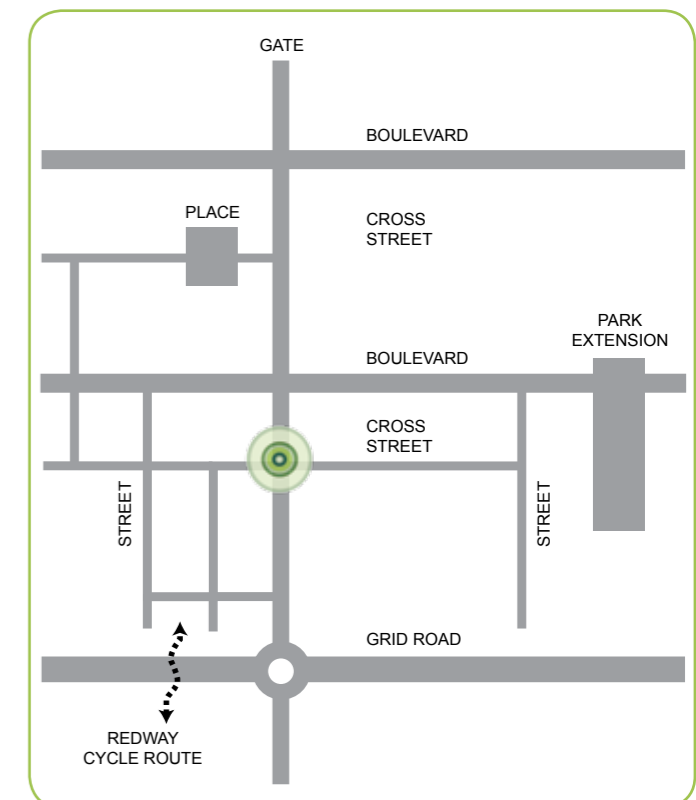
The Main Features

- 1 Convenient crossing of Gate.
- 2 Active building fronts.
- 3 Symmetrical lighting layout achieved.
- 4 No guard railing.
- 5 Trees extend up to the junction.

Gate / Cross Street Approach from Grid Road Central Reservation closed



Vehicle safety is maximized by preventing crossing movements at the junction. Cyclists are guided away from the 'Grid' roads using the minimum amount of carriageway markings. Pedestrian crossing facilities are created. The illustration shows a staggered crossing opposite the approach from the Grid Road (the quieter side of the junction).



Gate / Cross Street

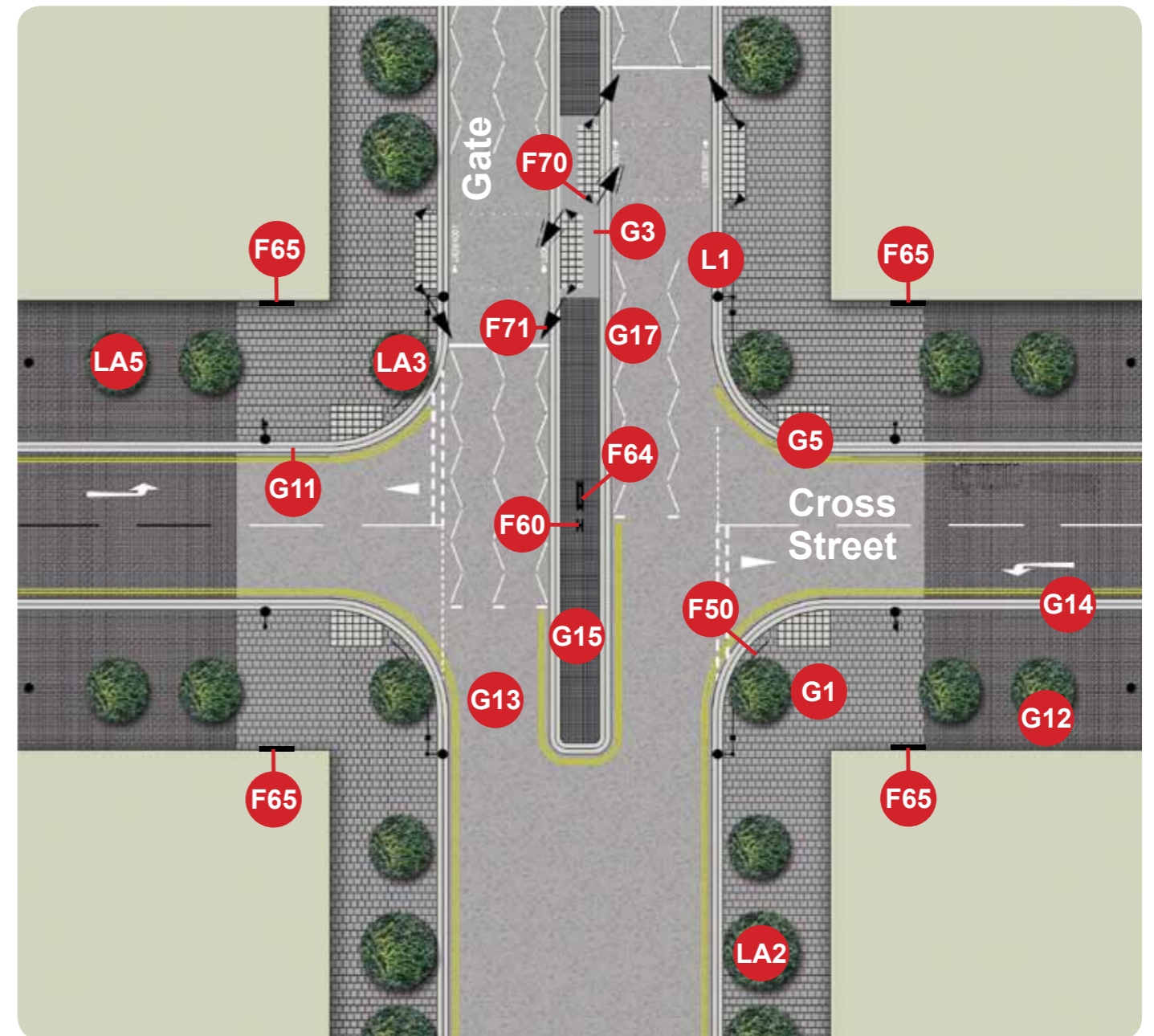
Central Reservation closed

Setting out Principles

- | Lighting - 0.8m offset to kerb to suit traffic signal equipment (where mounted).
- | Lighting – positioned symmetrically on kerbside of footways.
- | Pedestrian crossings – 4m wide.
- | Kerb radii – retained at 6m.
- | Controlled pedestrian crossing – set out on near side of line of existing lighting columns.
- | Uncontrolled pedestrian crossings – set out on line of footway.
- | Gate central reservation – minimum 2.5m wide for controlled crossing.
- | Left turn signs – mounted back to back.
- | Zig-zag markings – maximum eight.
- | Footways-widths (from back of footway to front of kerb) are:
 - | Gates 9.5m
 - | Cross Street 6.3m

Public Realm Elements

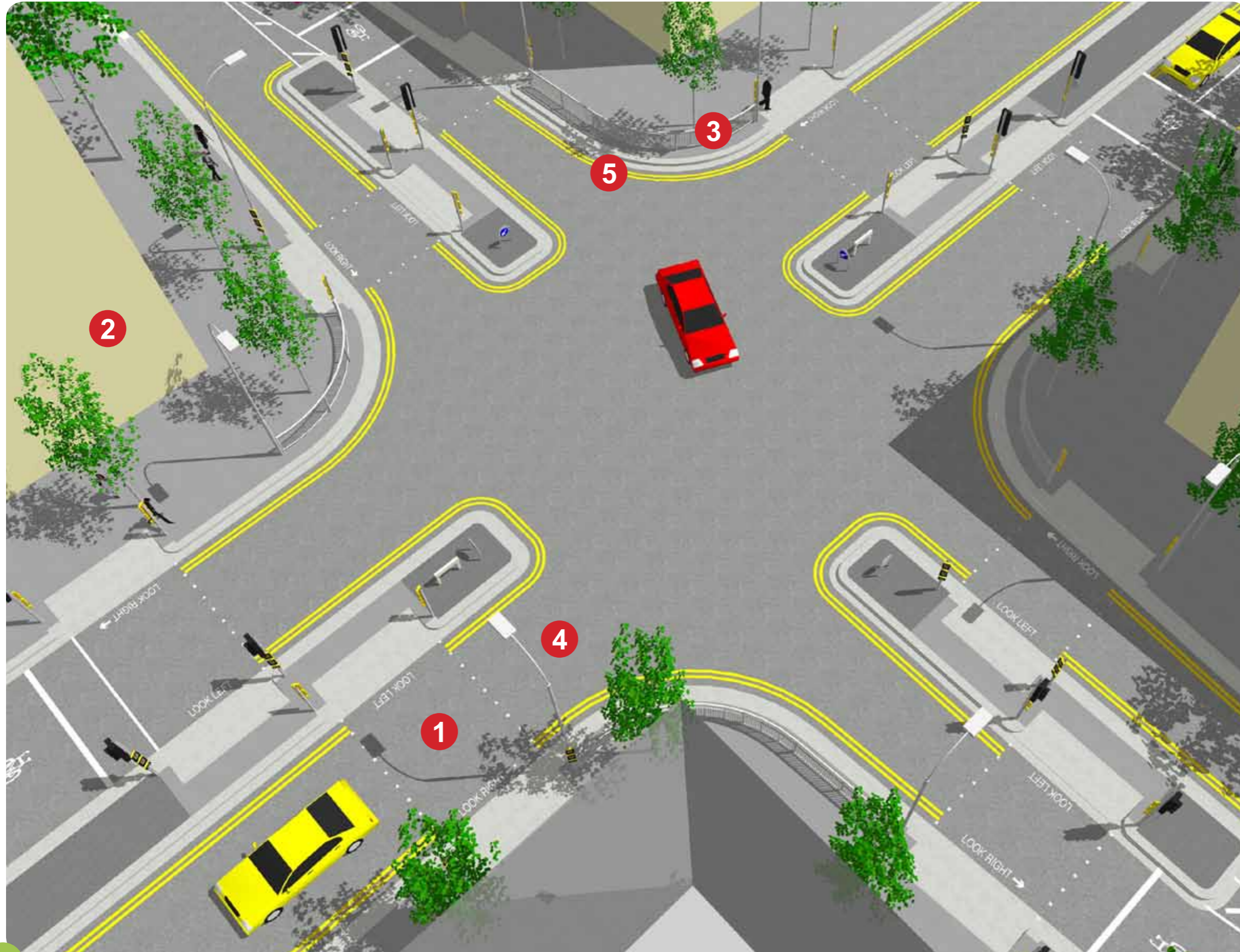
- G1** Silver grey pre-cast concrete paving flags 400 x 400 stagger bond
- G3** Concrete setts 100 x 100 staggerbond
- G5** Precast concrete tonal contrast tactile paving 400 x 400 stack bond – three full rows and no tails
- G11** Light silver-grey granite kerbs
- G12** Drainage channel with bright steel grating.
- G13** Black matrix hot-rolled asphalt
- G14** Precast concrete blocks mid-grey colour 200 x 100 herringbone bond with contrasting colour at crossing points
- G15** Granite setts 100 x 100 stack bond
- G17** Durable paint carriageway markings
- F50** Painted black guard rail
- F60** Stainless steel with lacquer finish traffic sign with internally illuminated display
- F64** Steel street name
- F65** Steel side street name
- F70** Stainless steel with lacquer finish traffic signal pole with near-side vehicle and pedestrian aspects
- F71** Stainless steel with lacquer finish traffic signal pole with near-side pedestrian aspects
- L1** 8m tall painted lacquered galvanised steel column with aluminium lantern.
- LA2** Fastigate Hornbeam *Carpinus betulus* 'Frans Fontaine' at 4.8m spacing
- LA3** Ornamental Pear *Pyrus calleryana* 'Chanticleer' or similar as agreed
- LA5** Small leaved Lime *Tilia cordata* 'Greenspire' or similar as agreed



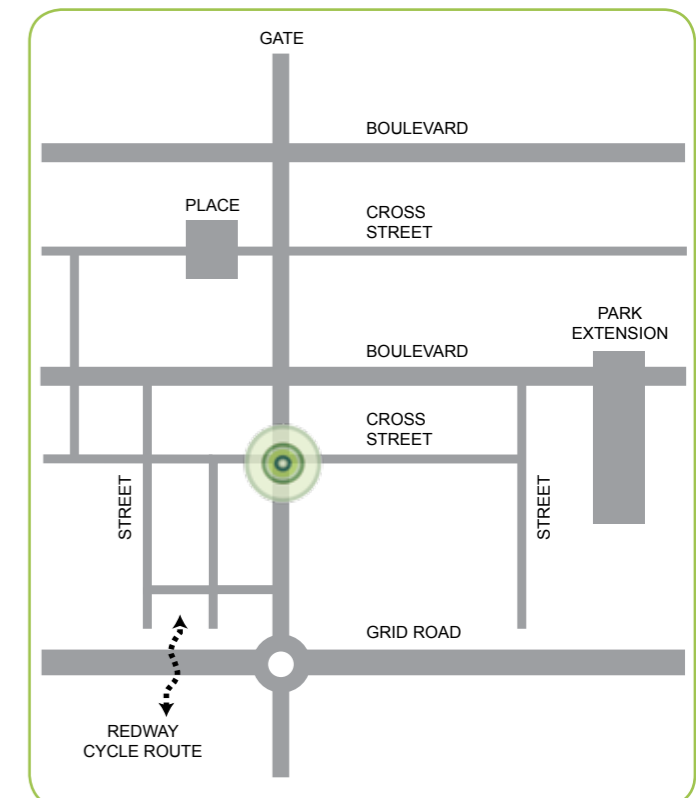
The Main Features

- 1 Convenient crossing of Gate.
- 2 Active building fronts.
- 3 Street furniture integrated at every opportunity.
- 4 Symmetrical lighting layout achieved.
- 5 Minimum guard railing.

Gate / Cross Street Approach from Grid Road Central Reservation open



All vehicle movements are permitted, which avoids the need for U-turns at other junctions and unnecessary driving. Pedestrian crossing facilities are created on every arm of the junction – the staggered arrangement marginally favours vehicles over pedestrians, but the minimum guardrail allows easy movement on foot. The islands are provided on the minor arms of the junction to facilitate pedestrian movements without an ‘all red’ stage, so that traffic can flow freely.



Gate / Cross Street

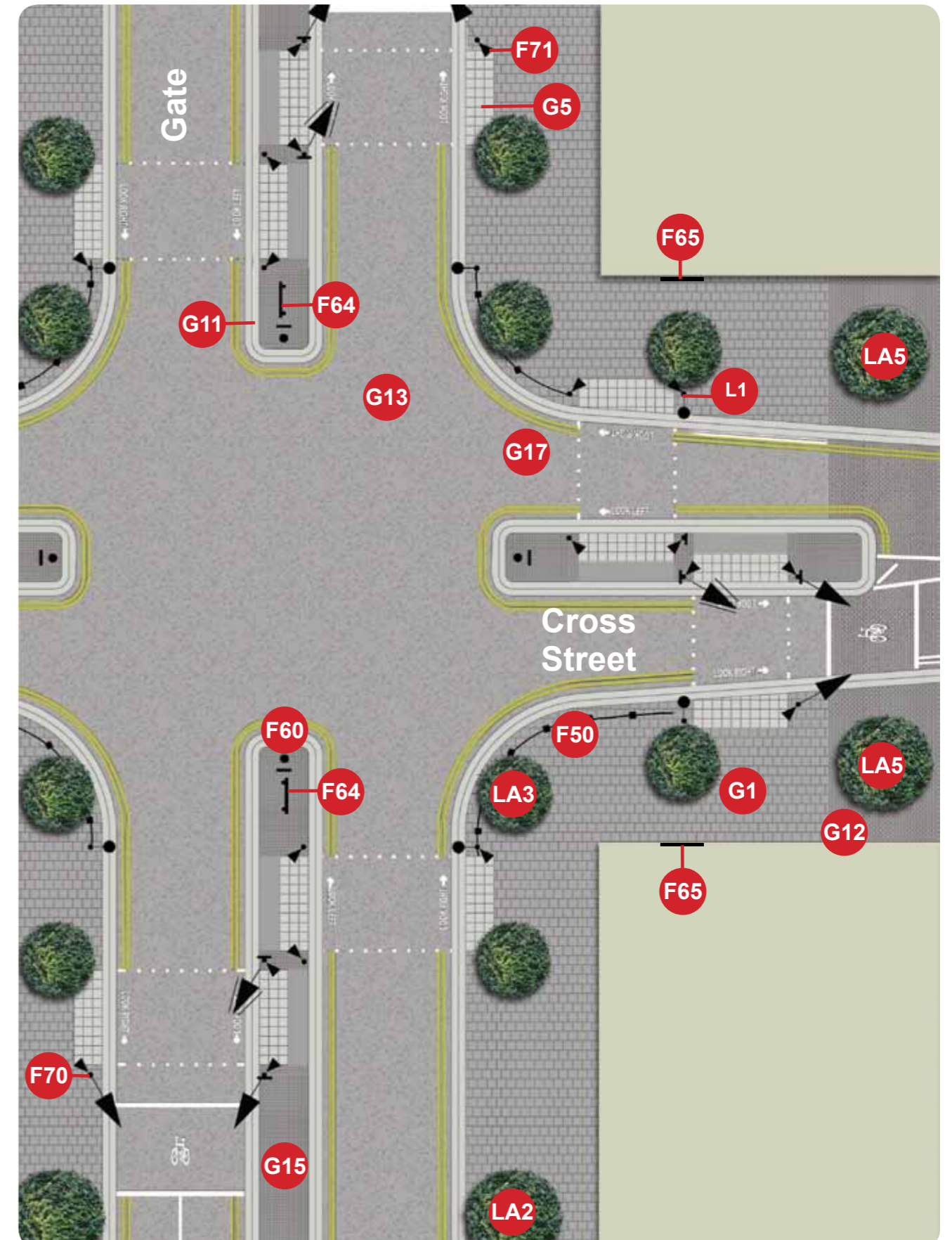
Central Reservation open
Staggered Crossing

Setting out Principles

- Lighting - 0.8m offset to kerb to suit traffic signal equipment (where mounted).
- Lighting – positioned symmetrically on kirbide of footways.
- Pedestrian crossings – 4m wide.
- Staggered pedestrian crossings – set out either side of line of existing lighting columns.
- Staggered pedestrian crossings – set out to achieve symmetrical poles and lighting columns.
- Straight pedestrian crossings – set out on the far side of line of existing lighting columns.
- Gate central reservation – minimum 2.5m wide.
- Traffic signal equipment – on central reservations, clear of line of pedestrian crossings.
- Keep left signs – on centre-line of central reservations.
- Footways-widths (from back of footway to front of kerb) are:
 - Gates 9.5m
 - Cross Street 6.3m
- Street splay dimensions must accommodate 3m carriageway width at the stop line.

Public Realm Elements

- G1** Silver grey pre-cast concrete paving 400 x 400 stack bond
- G3** Concrete setts 100 x 100 staggerbond
- G5** Precast concrete tonal contrast tactile paving 400 x 400 stack bond – three full rows and no tails
- G11** Light silver-grey granite kerbs
- G12** Drainage channel with bright steel grating
- G13** Black matrix hot-rolled asphalt
- G15** Granite setts 100 x 100 stack bond
- G17** Durable paint carriageway markings
- F50** Painted black guard rail
- F60** Stainless steel with lacquer finish traffic sign frontlit
- F64** Steel street name
- F65** Steel side street name
- F70** Stainless steel with lacquer finish traffic signal pole with near-side vehicle aspects
- F71** Stainless steel with lacquer finish traffic signal pole with near-side pedestrian aspects
- L1** 8m tall painted and lacquered galvanised steel column with aluminium lantern
- LA2** Fastigate Hornbeam *Carpinus betulus* ‘Frans Fontaine’ at 4.8m spacing
- LA3** Ornamental Pear *Pyrus calleryana* ‘Chanticleer’ or similar as agreed
- LA5** Small Leaved Lime *Tilia cordata* ‘Greenspire’ or similar as agreed



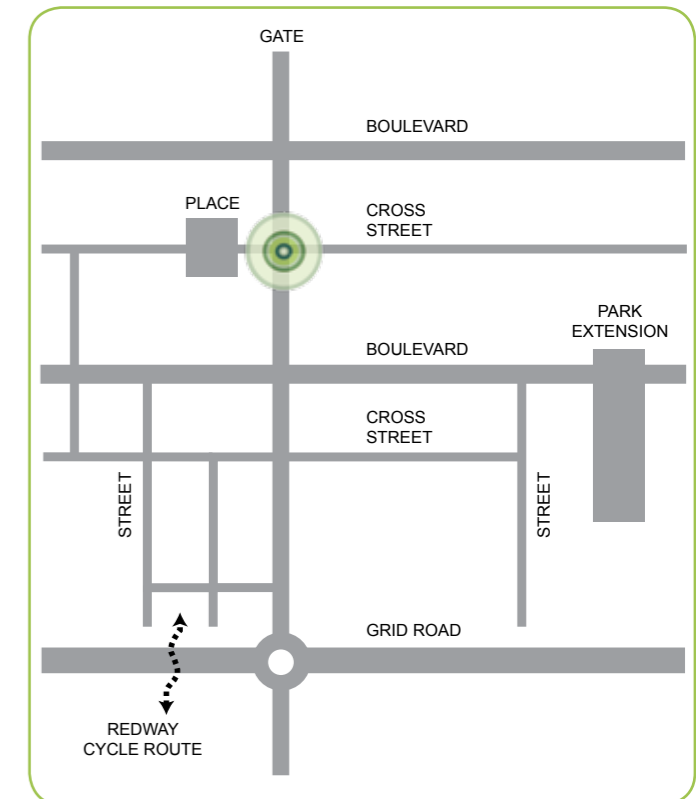
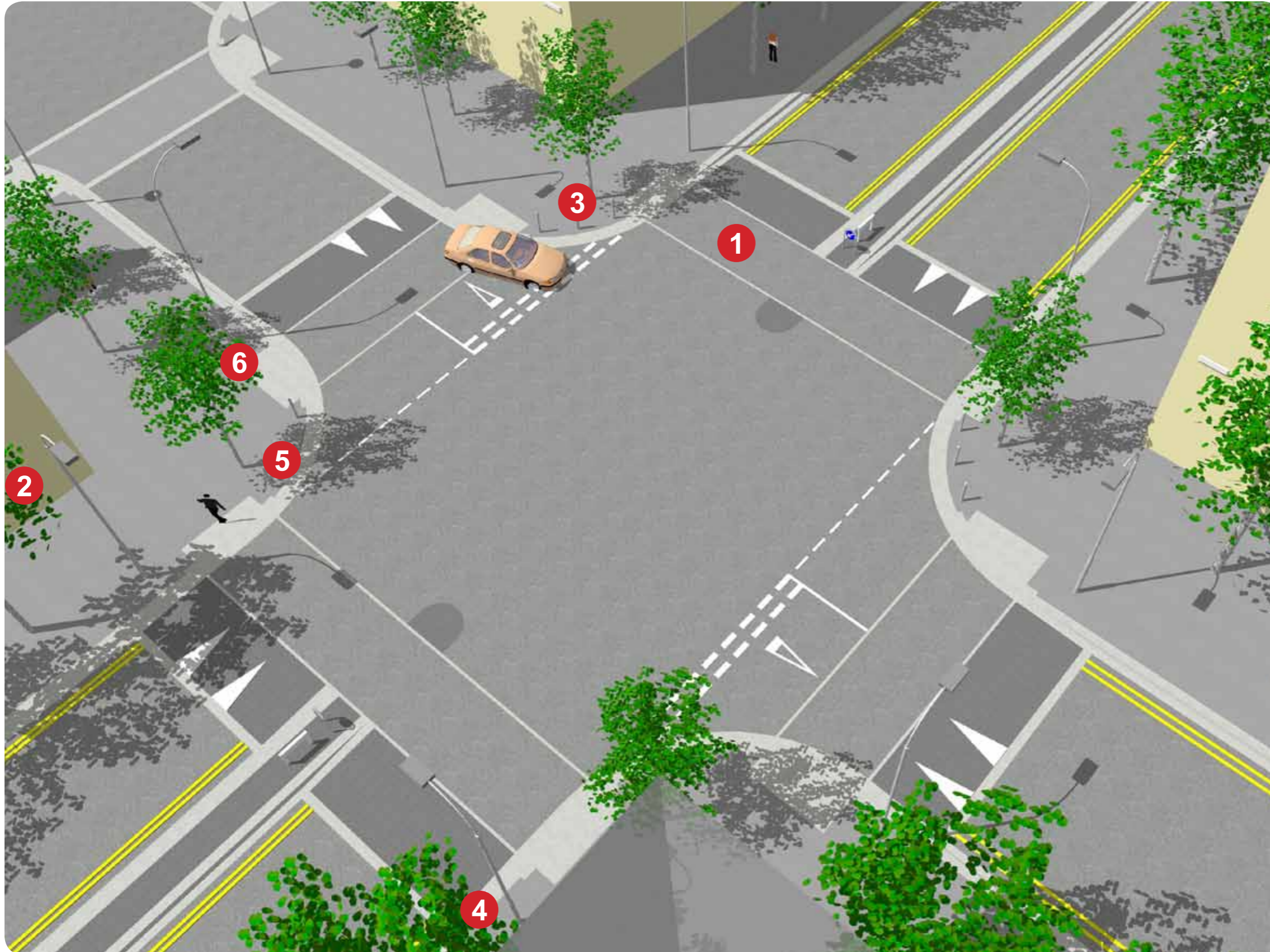
The Main Features

- 1 Convenient direct uncontrolled crossings on raised tables.
- 2 Active building fronts.
- 3 Minumim street furniture.
- 4 Symmetrical lighting layout achieved.
- 5 No guard railing.
- 6 Trees extend up to the junction.

Gate / Cross Street

Central Reservation open

The raised table offers higher priority to pedestrians than vehicles. Pedestrians can now cross the Gate comfortably along cross-streets, without having to deviate to Portes-Cochere crossing points.



Gate / Cross Street

Central Reservation open

Setting out Principles

- Lighting - 0.8m offset to kerb to suit traffic signal equipment (where mounted).
- Lighting – positioned symmetrically on kerbside of footways.
- Pedestrian crossings – 4m wide.
- Staggered pedestrian crossings – set out either side of line of existing lighting columns.
- Staggered pedestrian crossings – set out to achieve symmetrical poles and lighting columns.
- Straight pedestrian crossings – set out on the far side of line of existing lighting columns.
- Gate central reservation – minimum 2.5m wide.
- Traffic signal equipment – on central reservations, clear of line of pedestrian crossings.
- Keep left signs – on centre-line of central reservations.
- Footway widths allow for 2.4m width for laybys.
- Footways- widths (from back of footway to front of kerb) are:
 - Gates 9.5m
 - Cross Street 6.3m

Public Realm Elements

- G1** Silver grey pre-cast concrete paving flags 400 x 400 stagger bond
- G5** Precast concrete tonal contrast tactile paving 400 x 400 stack bond – three full rows and no tails
- G11** Light silver-grey granite kerbs to top and bottom of ramps – laid to ends on bottom and to faces on top
- G12** Drainage channel with bright steel grating
- G13** Black matrix hot-rolled asphalt
- G14** Mid-grey concrete block work paving 200 x 100 herringbone bond
- G15** Granite setts 100 x 100 stack bond
- G17** Durable paint carriageway markings
- G21** Raised table formed from concrete blocks 200 x 100 herringbone bond to ramps and black matrix hot-rolled asphalt to tops
- G22** Laybys formed from Pre-cast concrete blocks 200mm x 100mm Herringbone bond
- F10** Stainless steel bollard
- F60** Stainless steel with lacquer finish traffic sign with internally illuminated display
- F64** Steel street name
- F65** Steel side street name
- L1** 8m tall painted and lacquered galvanised steel column with aluminium lantern
- LA2** Fastigate Hornbeam *Carpinus betulus* 'Frans Fontaine' at 4.8m spacing
- LA3** Ornamental Pear *Pyrus calleryana* 'Chanticleer' or similar as agreed
- LA5** Small leaved Lime *Tilia cordata* 'Greenspire' or similar as agreed

