

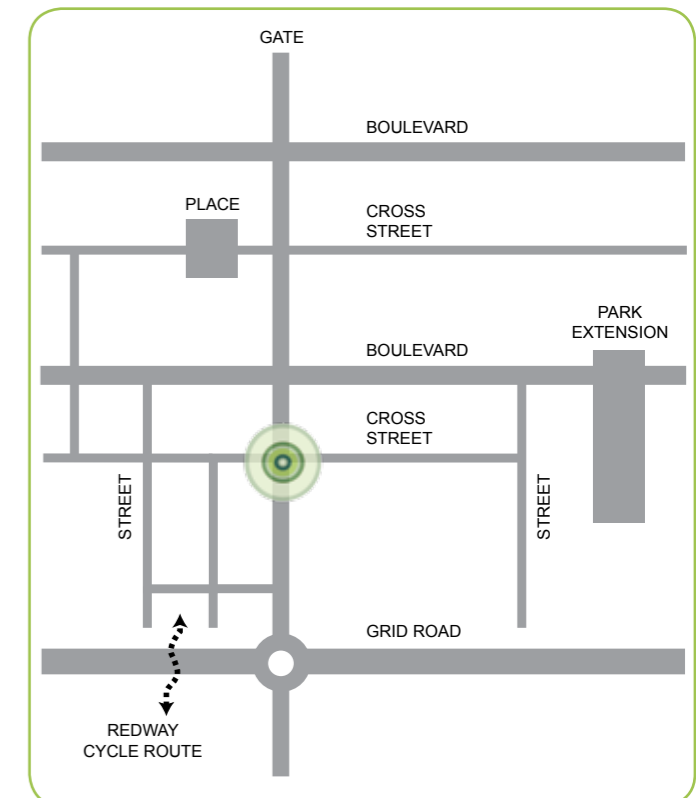
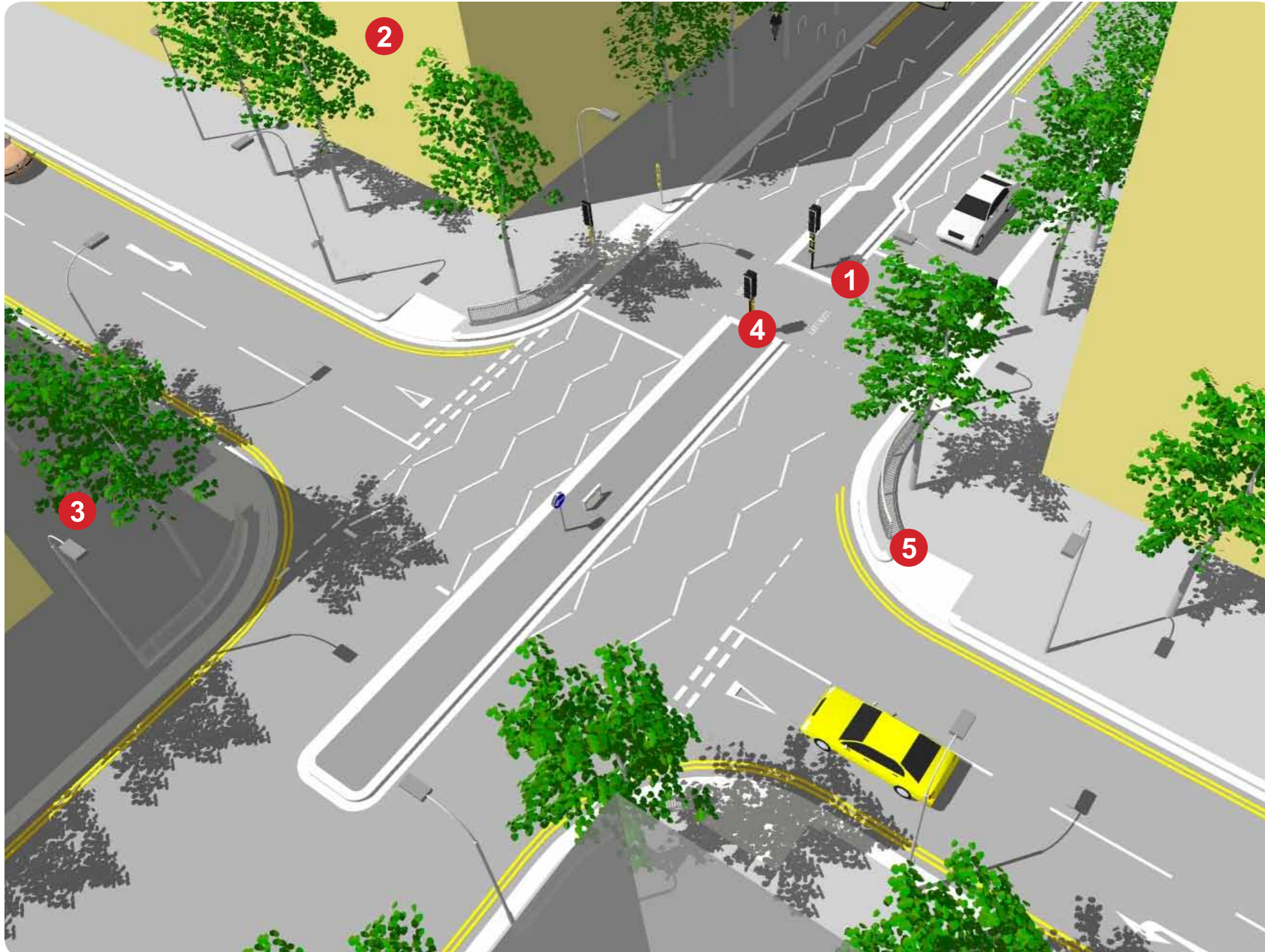
The Main Features

- 1 Straight crossing of Gate.
- 2 Active building fronts.
- 3 Symmetrical lighting layout achieved.
- 4 No guard railing.
- 5 Trees extend up to the junction.

Gate / Cross Street

Approach from Grid Road
Central Reservation closed
& straight pedestrian crossing

Vehicle safety is maximized by preventing crossing movements at the junction. Cyclists are guided away from the 'Grid' roads using the minimum amount of carriageway markings. Straight pedestrian crossing facilities are created; the illustration shows a straight crossing on approach from the Grid Road, (the quieter side of the junction).



Gate / Cross Street

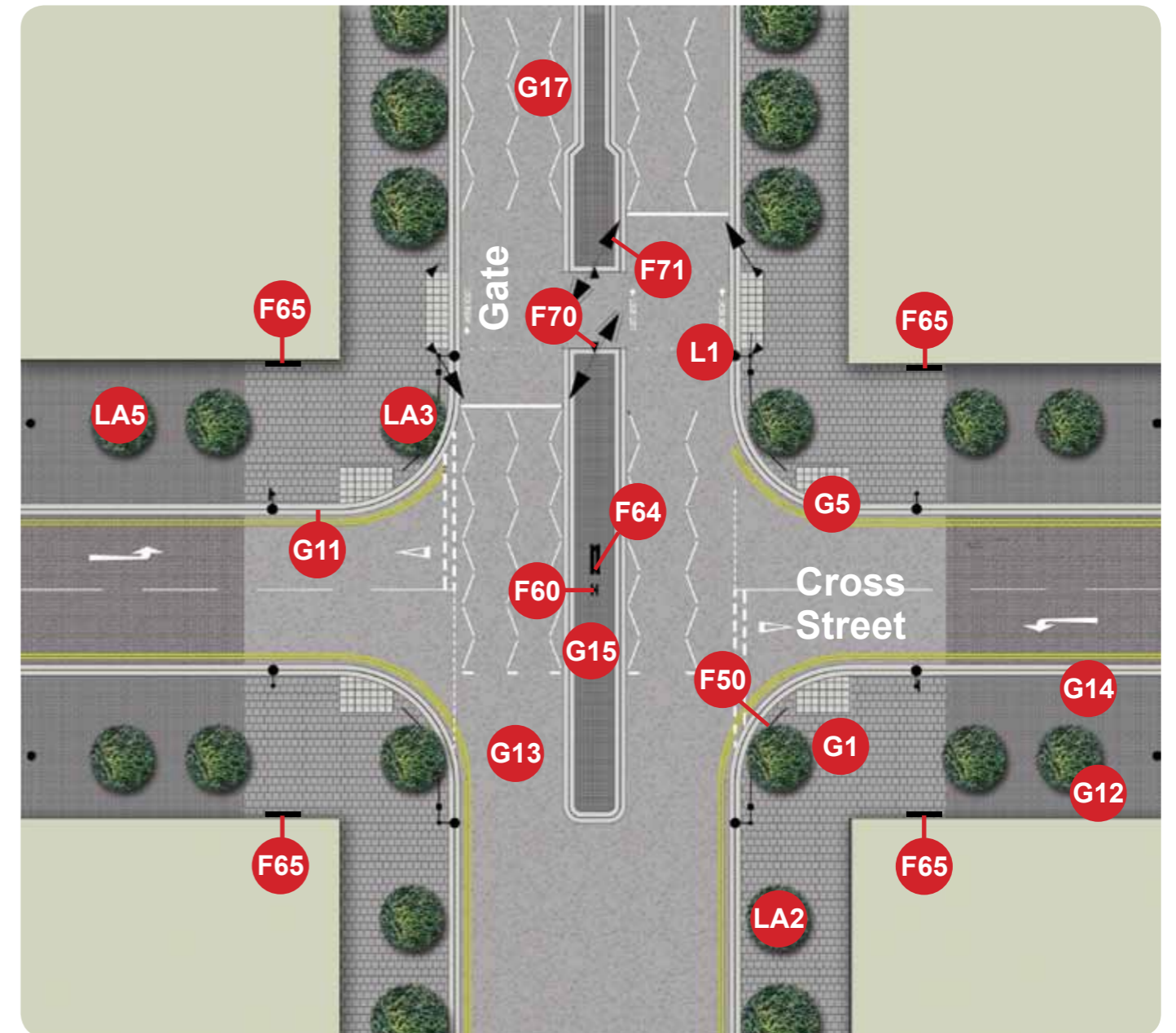
Approach from Grid Road
Central Reservation closed & straight crossing

Setting out Principles

- Lighting - 0.8m offset to kerb to suit traffic signal equipment (where mounted).
- Lighting – positioned symmetrically on kerbside of footways.
- Pedestrian crossings – 4m wide.
- Kerb radii – reduced to 6m.
- Controlled pedestrian crossing - set out on far side of junction from Grid Road.
- Controlled pedestrian crossing – set out on near side of line of existing lighting columns.
- Uncontrolled pedestrian crossings – set out on line of footway.
- Gate central reservation – minimum 2.5m wide for controlled crossing.
- Left turn signs – mounted back to back.
- Zig-zag markings – maximum eight.
- Footways-widths (from back of footway to front of kerb) are:
 - Gates 9.5m
 - Cross Street 6.3m

Public Realm Elements

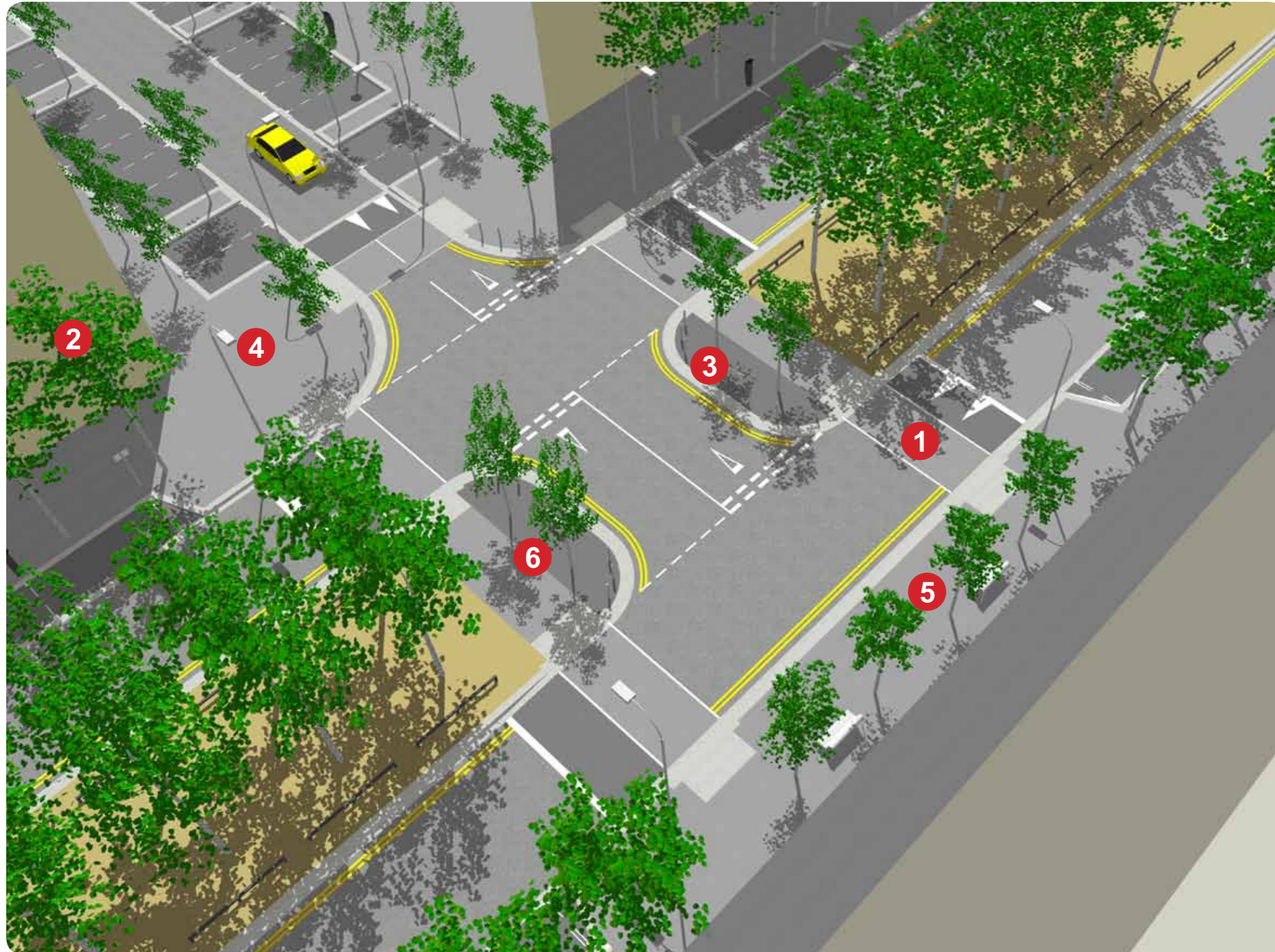
- G1** Silver grey pre-cast concrete paving flags 400 x 400 stagger bond
- G5** Precast concrete tonal contrast tactile paving 400 x 400 stack bond – three full rows and no tails
- G11** Light silver-grey granite kerbs
- G12** Drainage channel with bright steel grating
- G13** Black matrix hot-rolled asphalt
- G14** Precast concrete blocks mid-grey colour 200 x 100 herringbone bond with contrasting colour at crossing points
- G15** Granite setts 100 x 100 stack bond
- G17** Durable paint carriageway markings
- F50** Painted black guard rail
- F60** Stainless steel with lacquer finish traffic sign with internally illuminated display
- F64** Steel street name
- F65** Steel side street name
- F70** Stainless steel with lacquer finish traffic signal pole with near-side vehicle and pedestrian aspects
- F71** Stainless steel with lacquer finish traffic signal pole with near-side pedestrian aspects
- L1** 8m tall painted and lacquered galvanised steel column with aluminium lantern
- LA2** Fastigate Hornbeam *Carpinus betulus* ‘Frans Fontaine’ at 4.8m spacing
- LA3** Ornamental Pear *Pyrus calleryana* ‘Chanticleer’ or similar as agreed
- LA5** Small leaved Lime *Tilia cordata* ‘Greenspire’ similar as agreed



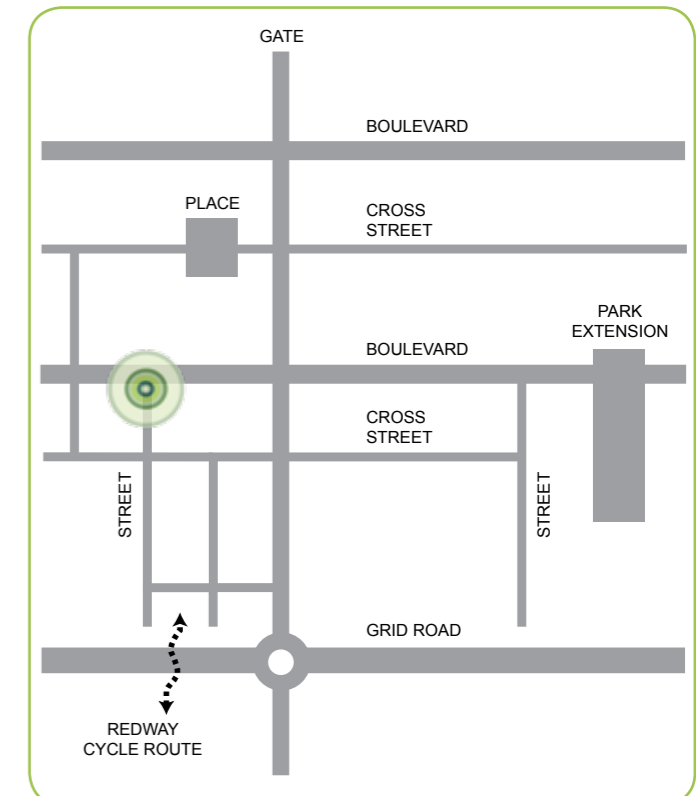
The Main Features

- 1 Convenient direct uncontrolled crossings on raised tables.
- 2 Active building fronts.
- 3 Minimum street furniture.
- 4 Symmetrical lighting layout achieved.
- 5 No guard railing.
- 6 Trees extend up to the junction.

Boulevard / Street Central Reservation open



The raised table offers higher priority to pedestrians than vehicles. Pedestrians can now cross the Boulevard comfortably along streets, without having to deviate to Porte-Cochere points.



Boulevard / Street

Central Reservation open

Setting out Principles

- Lighting - 0.8m offset to kerb to suit traffic signal equipment (where mounted).
- Lighting – positioned symmetrically on kerbside of footways.
- Pedestrian crossings – 4m wide.
- Staggered pedestrian crossings – set out either side of line of existing lighting columns.
- Staggered pedestrian crossings – set out to achieve symmetrical poles and lighting columns.
- Straight pedestrian crossings – set out on the far side of line of existing lighting columns.
- Boulevard central reservations – extended to position new light columns in line with other light columns.
- Boulevard central reservation 10m wide.
- Traffic signal equipment – on central reservations, clear of line of pedestrian crossings.
- Keep left signs – on centre-line of central reservations.
- Footways- widths (from back of footway to front of kerb) are:
 - Boulevards 9.5m
 - Streets minimum 3.3m

Public Realm Elements

- G1** Silver grey pre-cast concrete paving flags 400 x 400 stagger bond
- G2** Buff Breedon gravel
- G3** Concrete setts 100 x 100 stagger bond
- G5** Precast concrete tonal contrast tactile paving 400 x 400 stack bond – three full rows and no tails
- G11** Light silver-grey granite kerbs to top and bottom of ramps – laid to ends on bottom and to faces on top
- G12** Drainage channels with bright steel grating
- G13** Black matrix hot-rolled asphalt
- G14** Mid-grey concrete blockwork paving 200 x 100 herringbone bond
- G21** Raised table formed from concrete blocks 200 x 100 herringbone bond to ramps and black matrix hot-rolled asphalt to tops
- F10** Stainless steel bollard
- F31** Stone plinth
- F52** Painted black central reservation barriers
- F64** Steel street name
- F65** Steel side street name
- F93** Planter
- L1** 8m tall painted and lacquered galvanised steel column with aluminium lantern
- LA1** London *Platanus acerifolia* at 4.8m spacing
- LA2** Fastigate Hornbeam *Carpinus betulus* 'Frans Fontaine' at 4.8m spacing
- LA3** Ornamental Pear *Pyrus calleryana* 'Chanticleer' or similar as agreed

