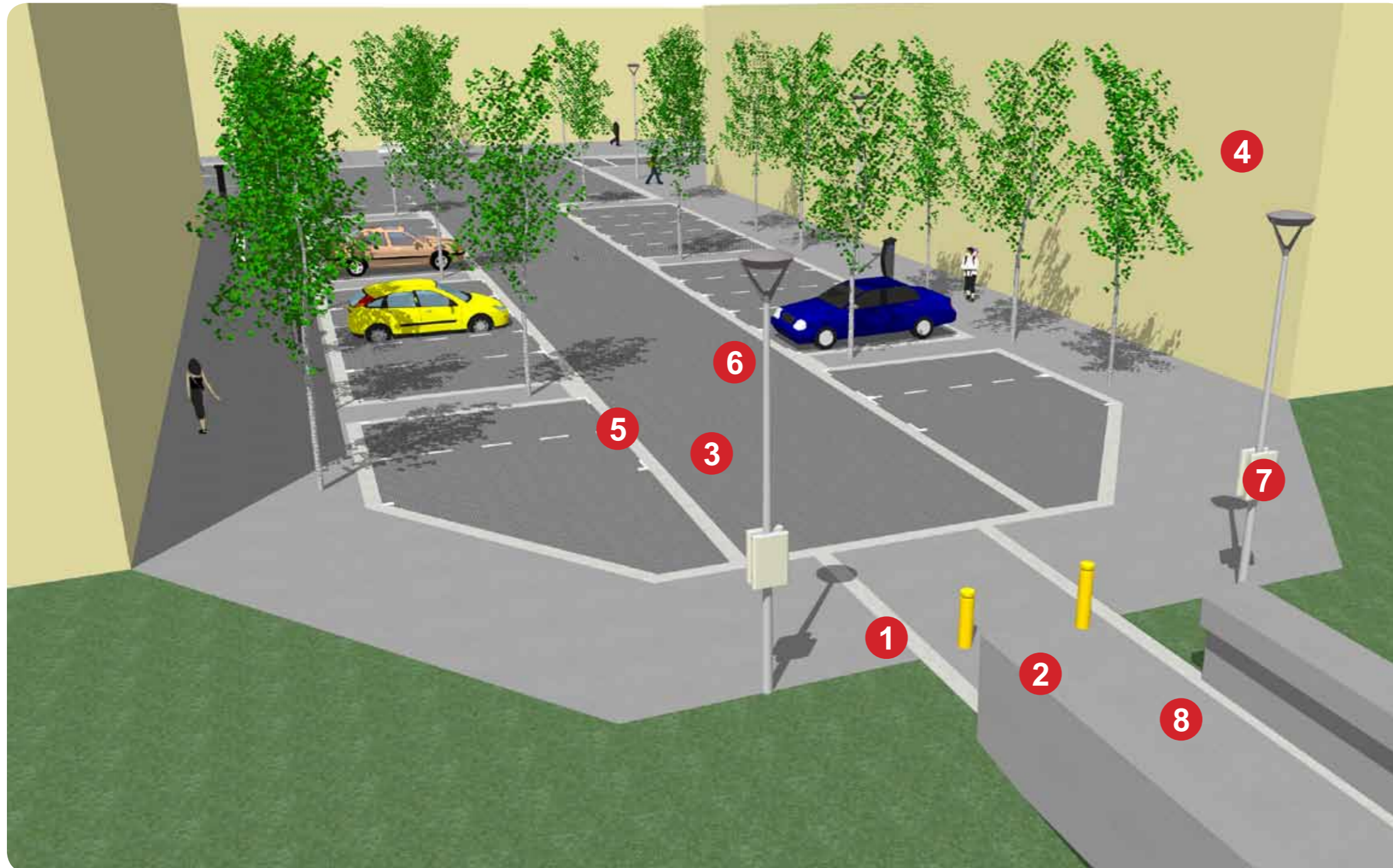


## The Main Features

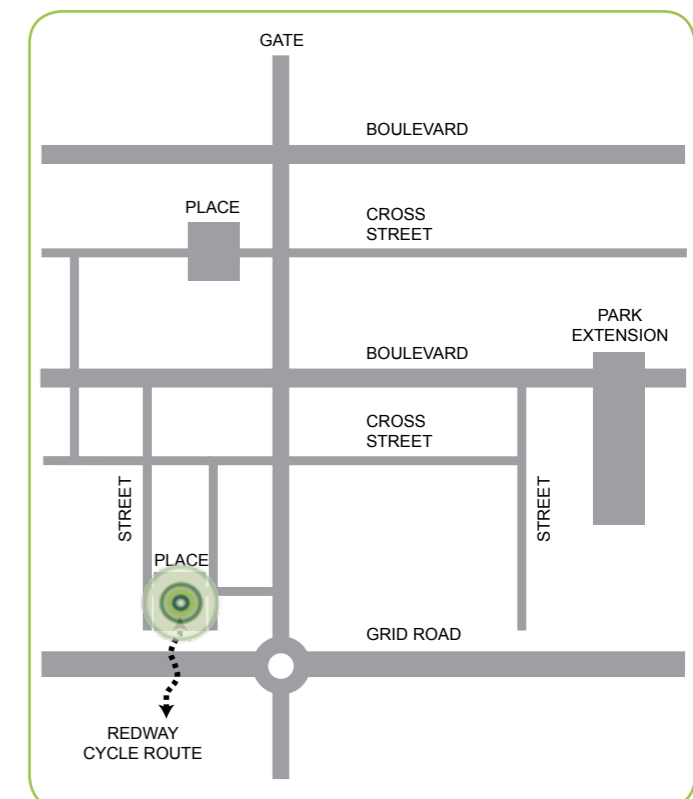
- 1 Gateway entrance feature to CMK.
- 2 Lighting close to approach/ exit points and foliage cut back to ensure redway approaches are well lit and visible.
- 3 Cycling integrated into street layout – no segregation.
- 4 Active building fronts.
- 5 Limited carriageway markings for guidance.
- 6 Slow speed environment.
- 7 Redway wayfinding information for cyclists.
- 8 Improvements to over bridge or underpass connections will be an integrated part of the design

## Street

### Redway/CMK Transition



Cyclists will be encouraged to use the carriageways in CMK, and this will be indicated on approach from the Redways as illustrated. Cyclists are integrated into a simple slow-speed environment, with a minimum of signage to guide them to the nearest Street. It is assumed that some of the existing Rows would be adapted to become more like Streets or Cross Streets, fronted by buildings on both sides. Pedestrians will have clear views crossing the Redways which will improve their feeling of personal security.



# Street

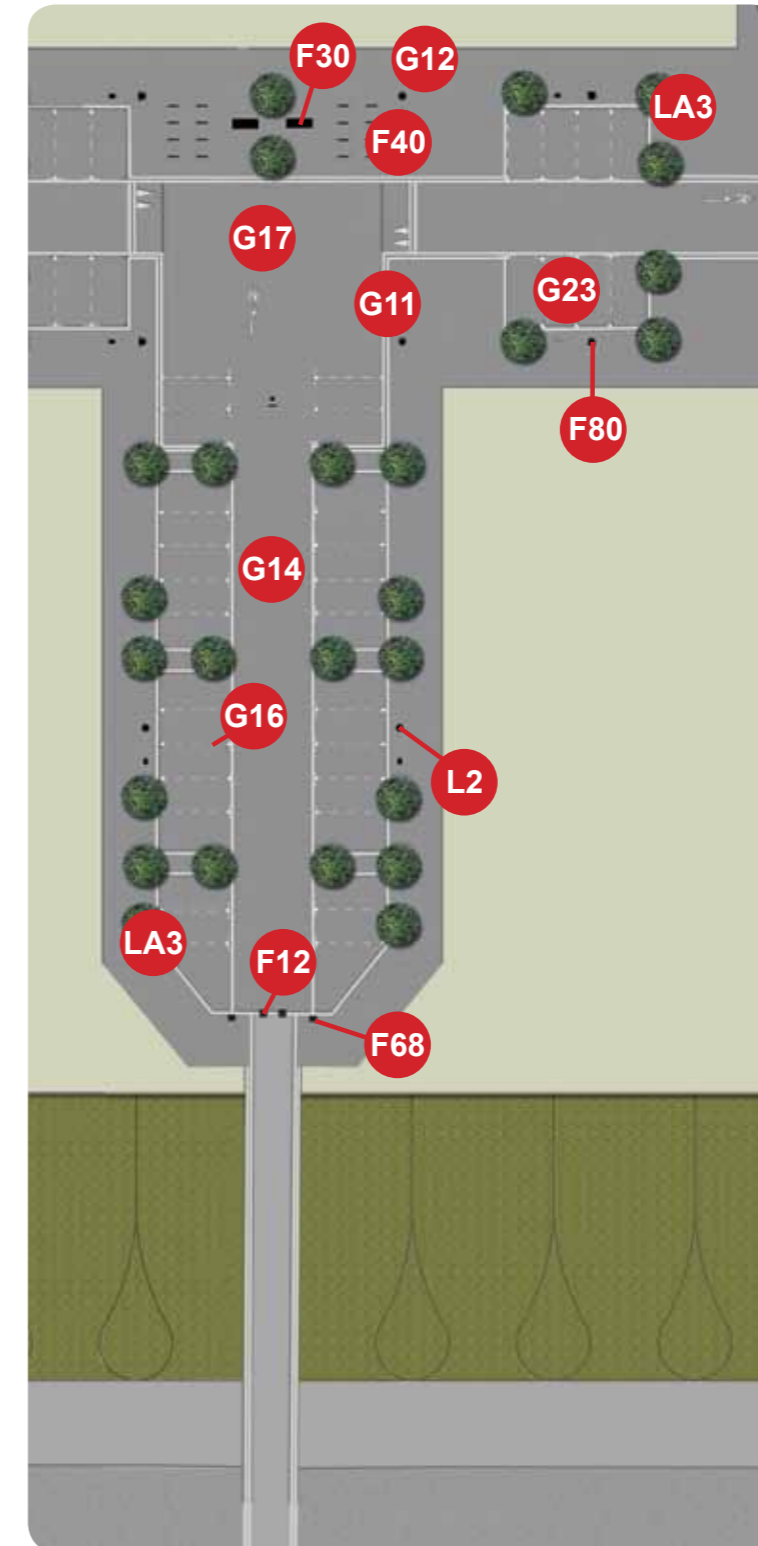
## Redway/CMK Transition

### Setting out Principles

- Lighting – setback from footways to match existing alignment on opposite side of Boulevard, Gate or Street.
- Lighting – 14.4m spacing to suit paving module and tree strategy.
- Trees – 4.8m spacing to suit paving module.
- Parking signs and parking machines – in line with trees and lights.
- Perpendicular parking bays- generally 2.5m wide, widening to 2.7m adjacent to tree lines to prevent damage to trees from opening car doors.
- Footways - width (from back of footway to front of kerb) are:
  - Streets minimum 3.3m

### Public Realm Elements

- G11** Light silver-grey granite kerbs to top and bottom of ramps
- G12** Drainage channels with bright steel grating
- G14** Mid-grey concrete blockwork paving 200 x 100 herringbone bond with contrasting colour at crossing points
- G16** White aggregate marker blocks 200 x 100 laid to 'T' shape
- G17** Durable paint carriageway markings
- G23** Parking bays formed from mid-grey concrete blockwork paving 200 x 100 herringbone bond
- F12** Painted yellow galvanized steel bollard
- F30** Stainless steel bench
- F40** Stainless steel cycle stand
- F68** Redway entry/ exit sign
- F80** Parking machine
- L2** 6m tall painted and lacquered galvanised steel column with aluminium lantern
- LA3** Ornamental Pear *Pyrus calleryana* 'Chanticleer' or similar as agreed

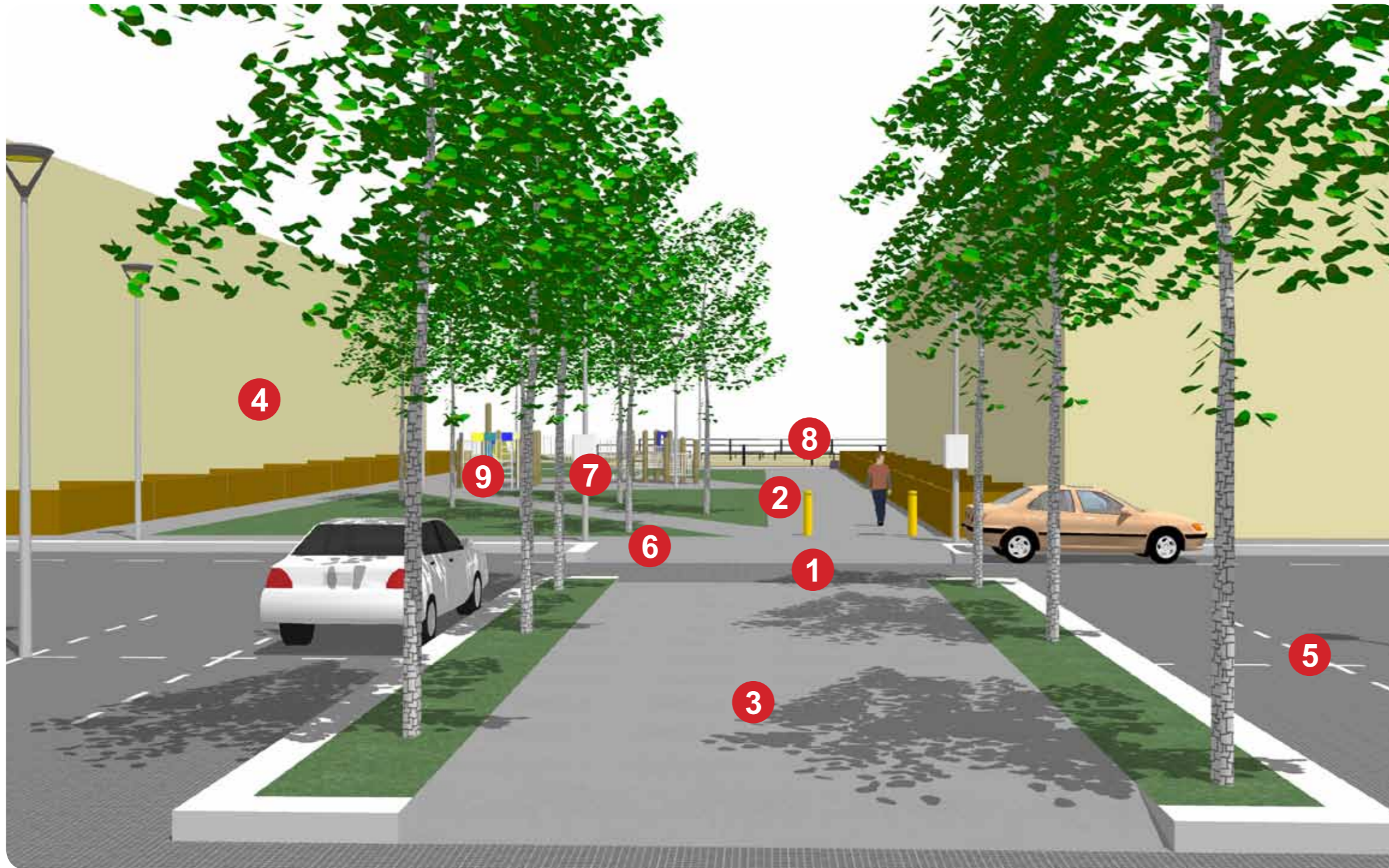


## The Main Features

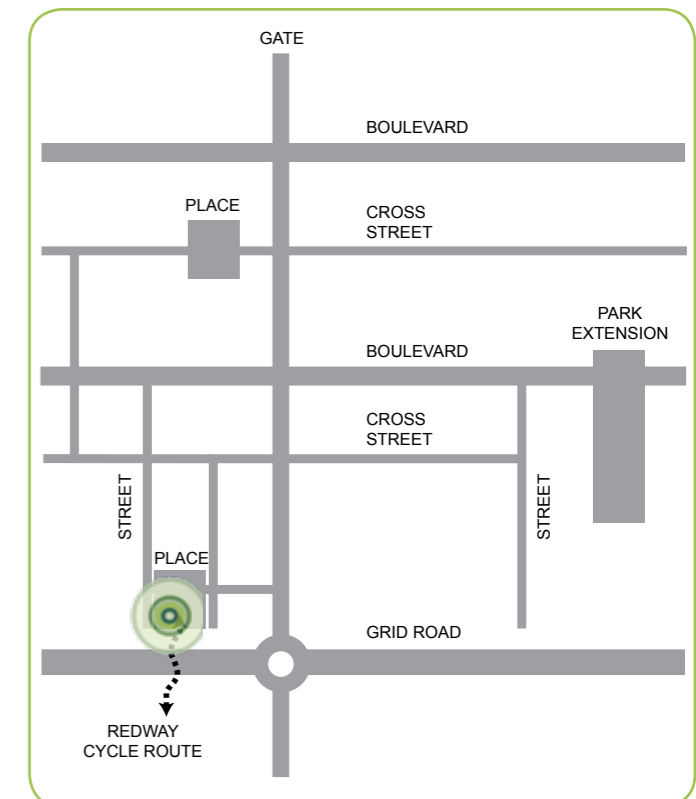
- 1 Gateway entrance feature to CMK.
- 2 Lighting close to approach/ exit points and foliage cut back to ensure redway approaches are well lit and visible.
- 3 Cycling integrated into street layout – no segregation
- 4 Active building fronts.
- 5 Limited carriageway markings for guidance.
- 6 Slow speed environment.
- 7 Redway wayfinding information for cyclists.
- 8 Improvements to overbridge and underpass connections will be an integrated part of the design
- 9 Hard and / or soft relaxation space that could include a play area

## Street

### Redway / Place Transition



It is assumed that some of the existing rows would be adapted to become more like Streets or Cross Streets, fronted by buildings on both sides. Pedestrians will have clear views crossing the Redway overbridge or underpass which will improve their feeling of personal security. Cyclists will be encouraged to use the carriageways in CMK, and this will be indicated on approach from the Redways as illustrated. Cyclists are integrated into a simple slow-speed environment, with a minimum of signage to guide them to the nearest Street.



# Street

## Redway / Place Transition

### Setting out Principles

- Lighting – setback from footways to match existing alignment on opposite side of Boulevard, Gate or Street.
- Lighting – 14.4m spacing to suit paving module and tree strategy.
- Trees – 4.8m spacing to suit paving module.
- Parking signs and parking machines – in line with trees and lights.
- Perpendicular parking bays- generally 2.4m wide, widening to 2.5m adjacent to tree lines to prevent damage to trees from opening car doors.
- Footways - width (from back of footway to front of kerb) are:
  - Streets minimum 3.3m

### Public Realm Elements

- G1** Silver Grey pre-cast concrete paving flags 400x400 stagger bond
- G5** Precast tonal contrast concrete tactile paving 400 x 400 - three full rows and no tails
- G11** Light silver-grey granite kerbs to top and bottom of ramps
- G12** Drainage channels with bright steel grating
- G14** Mid-grey concrete blockwork paving 200 x 100 herringbone bond with contrasting colour at crossing points
- G16** White aggregate marker blocks 200 x 100 laid to 'T' shape
- G17** Durable paint carriageway markings
- G23** Parking bays formed from mid-grey concrete blockwork paving 200 x 100 herringbone bond
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